

Handouts for The Public

City of Sandpoint Sidewalk Design Standards And Standard Drawings

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City of Sandpoint
Sidewalk Performance/Design Standards
Resolution 6-53 adopted July 19, 2006

I. The recommended Minimum Widths for Sidewalks are as follows:

Road Type (FHWA Classification)#	Principal Arterial (NHS)	Minor Arterial	Collector	Local Residential
Typical ROW	100 - 120	60 - 120	50 - 80	40 - 80
Typical Roadway Width	36 - 100	36 - 40	28 - 34	26 - 34
With planting strip buffer	6'	6'	5'*	5'*
With street trees no buffer	10'	10'	8'	
Urban Center	Full available ROW to curb when buildings abut ROW	Full available ROW to curb when buildings abut ROW		
Planting Buffer Width – The space between the sidewalk located on the edge of the ROW line and the curb				
* If obstructions are located within the sidewalk, provide a minimum clear width of 4'				

II. A. Common Elements: Sidewalks on both sides of the street; curb ramps at intersections; approved driveway and alley apron designs; cross slope a maximum of 2% (1/4" per foot) toward the street; slope in direction of travel 8.3% (1" per foot); approved street trees installed whenever practical; 8' minimum vertical clearance. Minimum 4' of sidewalk width to be completely clear of obstructions. Sidewalks shall be constructed along the right-of-way side of the property line. As much as possible, they will align with adjoining sidewalks.

B. In general, the width of a sidewalk or walkway needs to comfortably accommodate the volume of pedestrians normally using or anticipated to use it. The goal is to produce usable, accessible pedestrian facilities that serve users of all abilities and ages. Sidewalks may need to be wider than the minimum where there are designated Safe Routes to School walking routes, parks, recreation centers, transit stops, or other common pedestrian origins and destinations. The PW Director, Planning Commission or City Council may make that determination.

C. Sidewalks shall be constructed at grade with the top of the curb, unless approved by the City Engineer. The PW Director, Planning Commission or City Council may authorize deviation from the recommended minimum widths for compelling reasons, including, but not limited to, topography, available right of way, or existing building setbacks.

D. If necessary, roots of street trees should be buffered from the sidewalk to prevent future heave and buckle. Sidewalks may circumvent mature trees or outstanding landscaping features to the satisfaction of the City Engineer and landowner. Giving tree roots room helps prevent future maintenance problems. Trees are a desirable pedestrian amenity and efforts should be made to accommodate preexisting ones in or near the right of way and plant them in buffer strips. Sidewalk may veer onto adjoining private property if needed to avoid trees.

E. Existing subdivisions with existing 4' sidewalks are exempted from the 5' width requirement and allowed to remain 4': Northshore; Ponderosa Park and Ponderosa Park 1st Addition; and Sand Creek Place.

F. Where wider sidewalks are being constructed which abut a narrower sidewalk, the new sidewalk shall transition to the narrower sidewalk at an angle or curve approved by the Public Works Director.

III. The City adopts the # 2010 Sandpoint Urban Area and Functional Classification Map (signed by Federal Highway Administration representative, 1/14/04) which designates the following road types in Sandpoint:

- A. Principal Arterials/National Highway System (NHS):**
 Highway 2
 Cedar St. (1st to 5th)

5th Ave. (Pine to City of Ponderay)
1st Ave. (Superior to Cedar)
Pine St. (1st to Highway 2)
Superior (Long Bridge to 1st Ave.)

B. Minor arterials:

Baldy Mountain Road
Boyer Ave. (Highway 2 to Schweitzer Cutoff Road)
Division Ave (North of Hwy 2)
Pine St. (between Highway 2 and Division)
Schweitzer Cut-off Road

C. Collector:

Boyer Ave. (south of Highway 2)
N. Boyer Ave.
Bridge St.
Cedar St. (west of 5th)
Division Ave. (South of Highway 2)
Ella Ave. (Pine to Larch)
Euclid Ave
Great Northern Road
Larch St. (between Boyer and Division)
Lincoln Ave.
Main St. (between 5th and Division)
Olive Ave.
Ontario St.
Woodland Drive

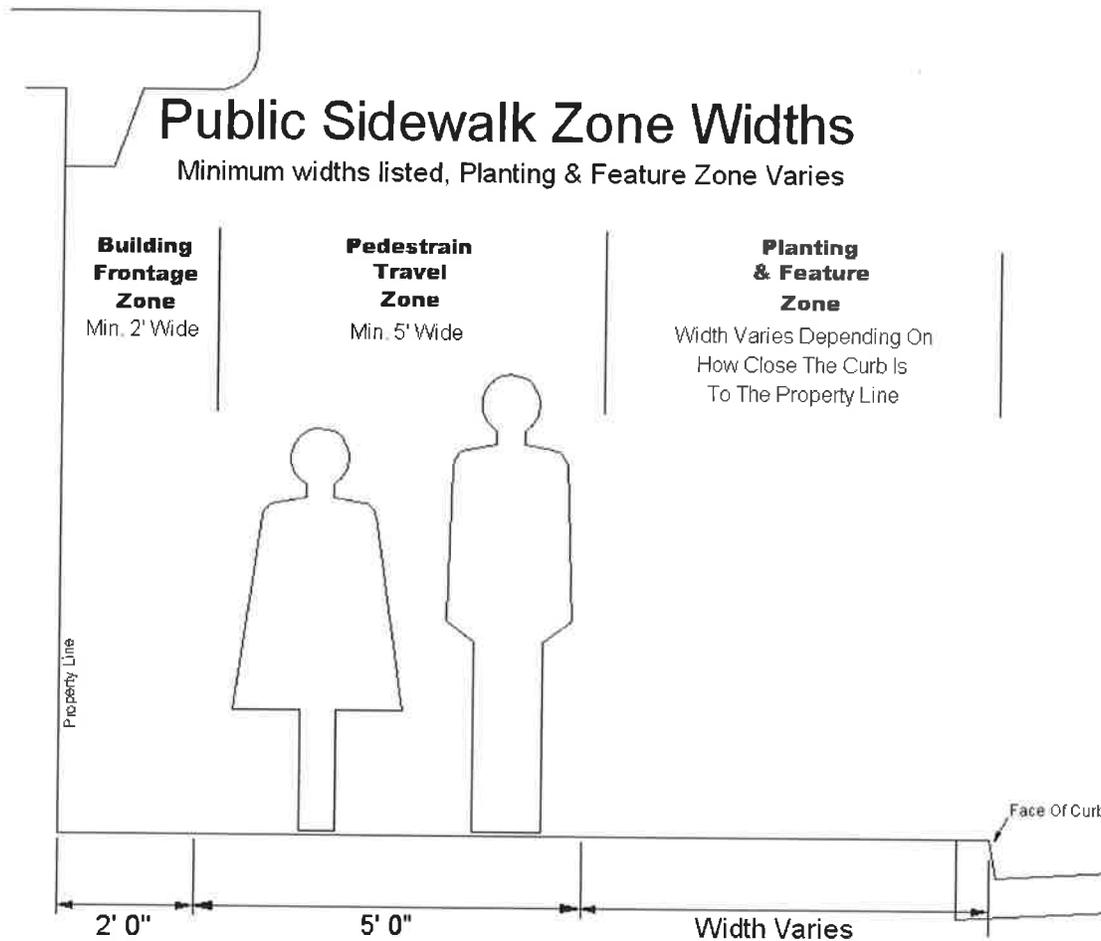
D. Local Residential:

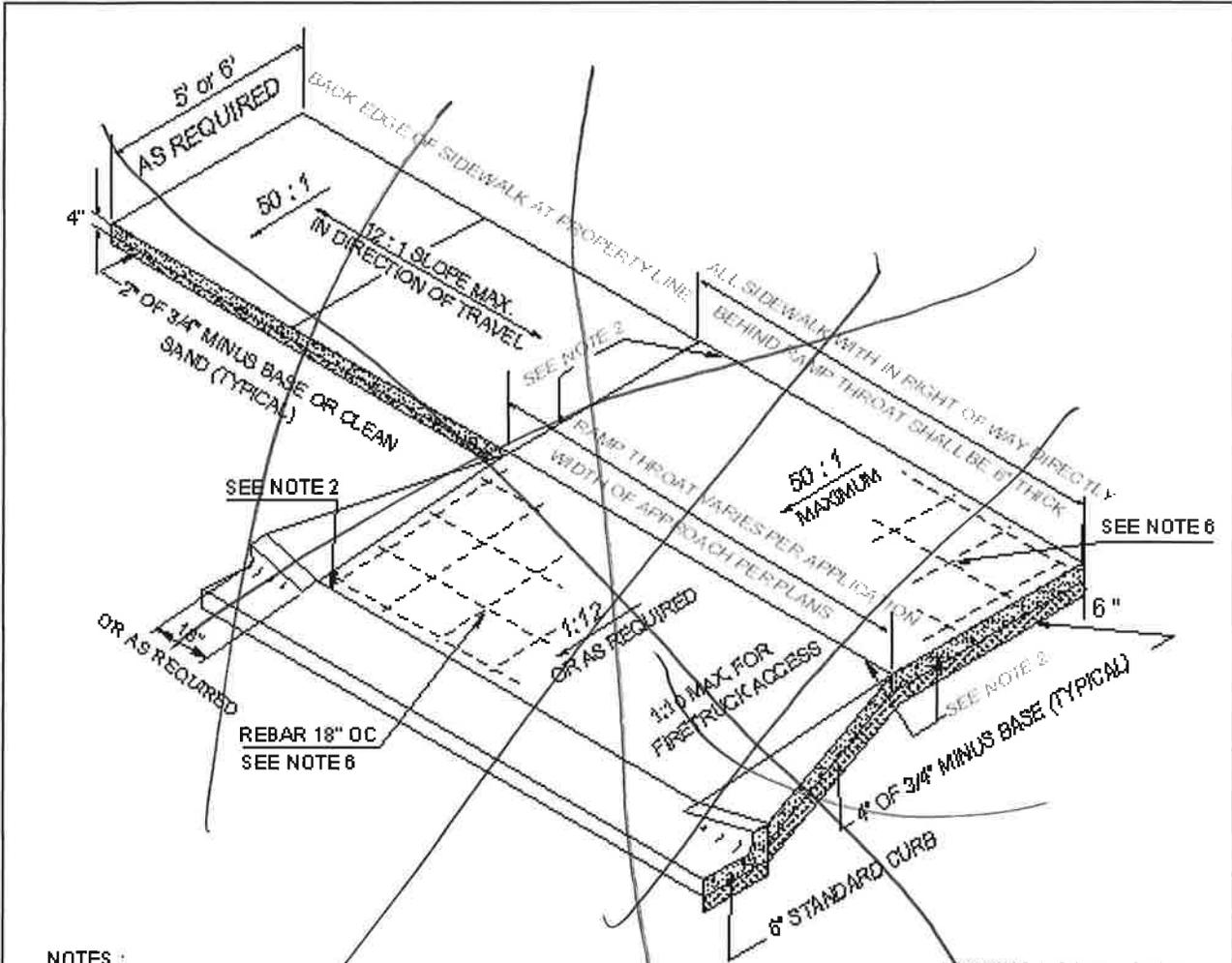
All other roads

End of Resolution 6-53

Pedestrian Configuration in Commercial/Downtown Areas

- The Pedestrian Travel Zone is a minimum of five (5) feet wide and begins at least two (2) feet from the building. The Pedestrian Travel Zone shall be free of impediments: signs, utility poles, seating, etc.
- The Planting & Features Zone lies between the Pedestrian Travel Zone and auto travel or parking. It is appropriate for seating, street trees, signs, etc.
- The Building Frontage Zone shall generally be clear for building entry and window shopping. Where greater than three (3) feet wide it may include seating to enhance the adjoining business. Any items placed in the Building Frontage Zone shall not encroach on the Pedestrian Travel Zone.
- Sidewalks in the Commercial / Downtown zone should extend from the curb to the Building Front (or property line). Building to the front line is strongly encouraged. Paving design shall be that approved for Downtown Revitalization Phase I 2003.





NOTES :

1. SCORE SIDEWALK AT INTERVALS NOT TO EXCEED 5 FEET SPACING
2. INSTALL 1/2" EXPANSION JOINT WHERE INDICATED.
3. BASE TO BE 4" THICK 3/4" MINUS CRUSHED AGGREGATE PER SECTION 802 OF THE ISPWC. SUB-BASE AND BASE TO BE COMPACTED PER SECTION 202 OF THE ISPWC.
4. APPROACH THROAT WIDTHS PER TABLE BELOW. ALL CONCRETE TO BE 6" THICK FROM TIP OF WING TO TIP OF WING UP TO THE EXPANSION JOINT. SIDEWALK BEHIND THROAT SHALL BE 6" THICK ALSO.
5. ALL CONCRETE SHALL BE CLASS 3000 PER SECTION 703 OF THE ISPWC.
6. 1/2" REBAR @ 18" O.C. BOTH WAYS IN 6" APRON & WALK IS REQUIRED, OR EQUIVALENT AS DETERMINED BY CITY.

DRAWING NOT TO SCALE

mod →

MAXIMUM WIDTHS	
RESIDENTIAL	20'
COMMERCIAL	24' - SHARED USE 36'
INDUSTRIAL	32' - SHARED USE 40'

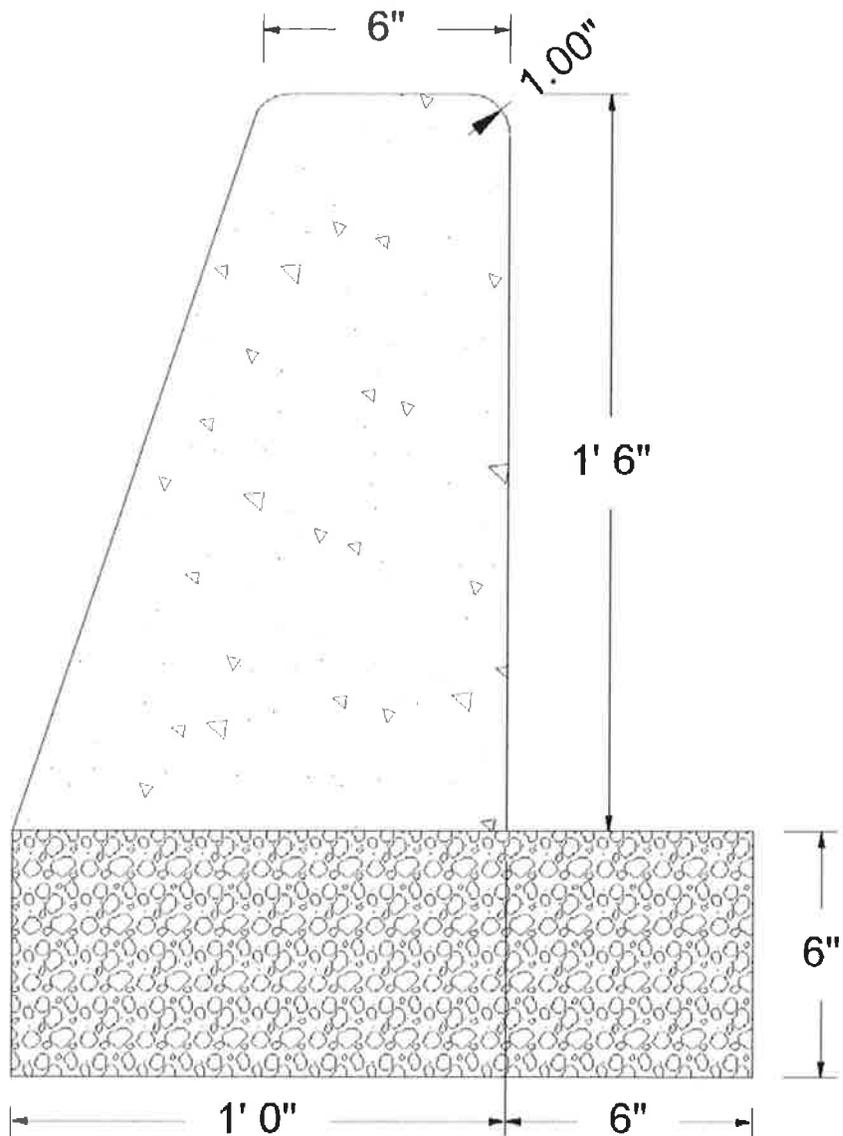
See Page 1 std. Sidewalk Detail S(12x17) last page

CONCRETE DRIVEWAY APPROACH

CITY OF SANDPOINT
STANDARD DRAWING

1/07

Crushed Rock Base



NOTES :

1. GRADE AND ALIGNMENT TO BE ESTABLISHED AND APPROVED BY THE ENGINEER AND THE CITY OF SANDPOINT.
2. BASE : 6" OF 3/4 MINUS CRUSHED AGGREGATE COMPACTED TO 95% OF STANDARD PROCTOR.
3. 1/2" PREFORMED EXPANSION JOINT MATERIAL AT TERMINAL POINTS OF RADII.
4. CONTINUOUS PLACEMENT PREFERRED
5. SCORE AT 10' INTERVALS MAX. (8' W/SIDEWALK)
6. MATERIALS AND CONSTRUCTION IN COMPLIANCE WITH ISPWC.
7. BACKFILL AS PER ISPWC SECTION 706
8. VERTICAL CURB IS ONLY TO BE USED TO MATCH EXISTING CURB

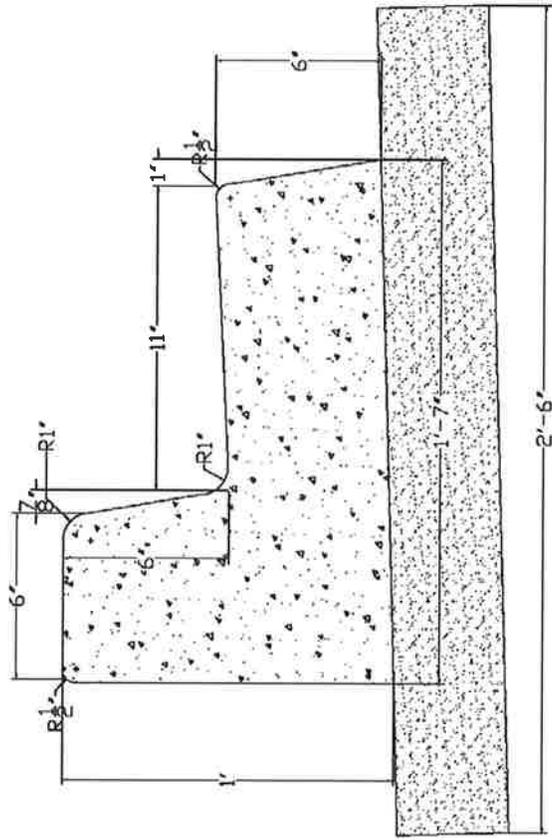
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Vertical Curb

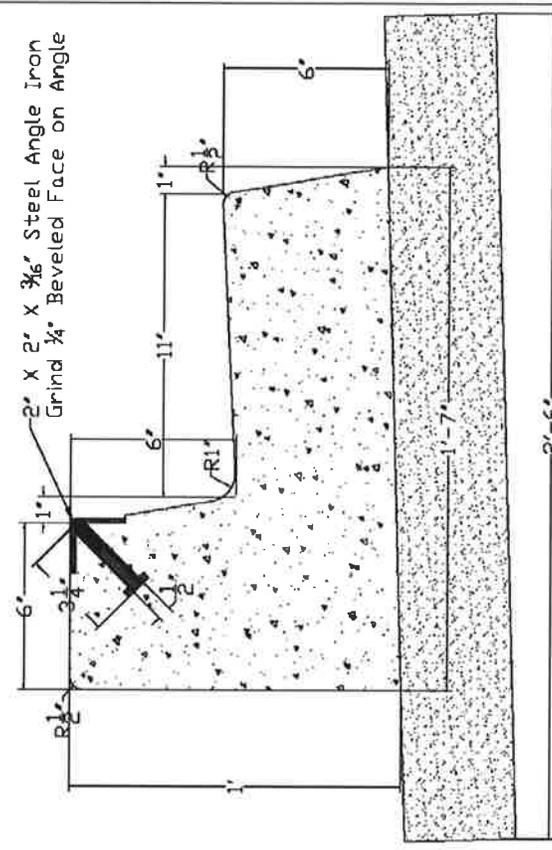
PROJECT: **City of Sandpoint
Standard Drawings**

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CITY OF SANDPOINT
PUBLIC WORKS



19" Curb & Gutter

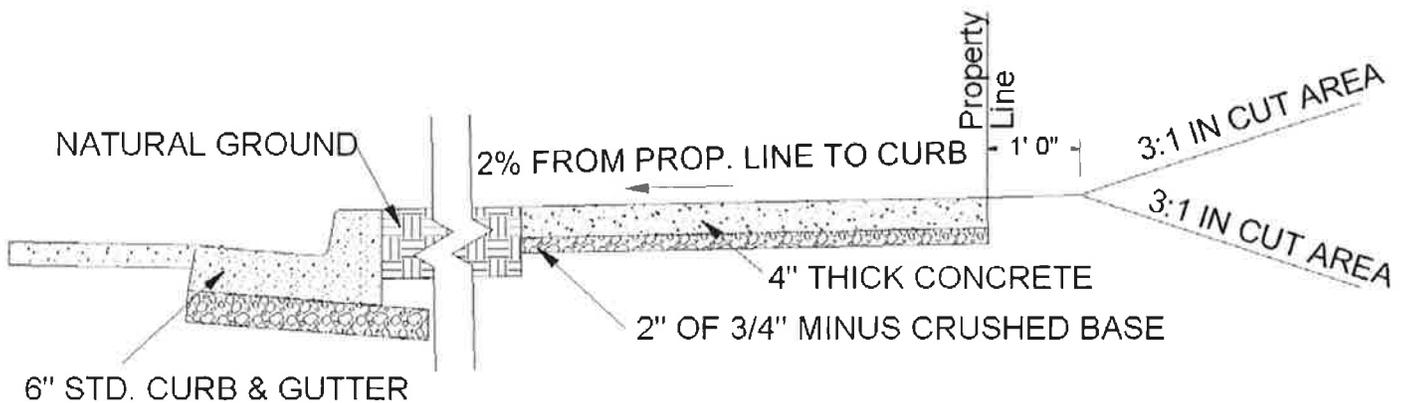
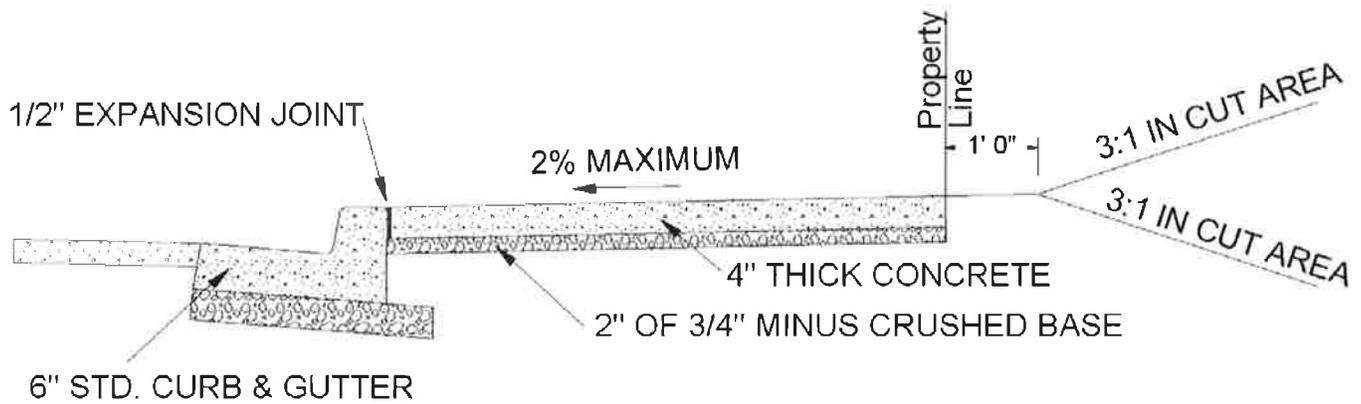


Armored Curb & Gutter

Notes:

1. Grade and alignment to be approved by the engineer and the City of Sandpoint
2. Base 4" of 3/4" minus crushed aggregate compacted to 95% of standard proctor
3. Base shall be a min. width of 2'6" to grade prior to setting forms
4. 1/2" preformed expansion joint material at terminal points of Radii
5. Continuous placement preferred, score at 10' intervals max. (8' with sidewalk)
6. Materials and construction in compliance with ISPWC
7. Backfill as per ISPWC section 705
8. Gutter counterslope max. 5% (1/2" per foot) at pedestrian ramps
9. Concrete anchors for steel angle @ 18" OC and 3" from ends
10. Steel angle 2" X 2" X 3/16" rolled to 20' concave or convex radius as required
11. Steel angle welded into one continuous piece prior to pouring concrete
12. Steel angle cleaned to be free from rust or scale prior to installation. No paint or galvanizing required.

Project	City of Sandpoint		Date 2/13/13
	1123 Lake St		Revision
Sheet Title	Sandpoint, ID B3864		Drawn by BWR
	PW Dept (200) 255-1877		



1. LOCATION, GRADE AND WIDTH TO BE ESTABLISHED BY THE CITY
2. BASE TO BE COMPACTED TO EXCEED 95% OF STANDARD DENSITY
3. SLOPE SIDEWALK TOWARD STREET NOT TO EXCEED 1/4" PER FOOT (0.02 FT)
4. SCORE AT INTERVALS NOT TO EXCEED 5 FOOT SPACING
5. THROUGH JOINTS WITH 1/2" PREFORMED BITUMINOUS JOINT AT 20' INTERVALS
6. 1/2" TRAVERSE PREFORMED BITUMINOUS JOINTS AT TERMINOUS POINTS FOR CURVE AND WHERE SIDEWALK IS PLACED NEXT TO PERMANENT FOUNDATION OR CURB
7. MATERIALS AND CONSTRUCTION PER CITY STANDARDS

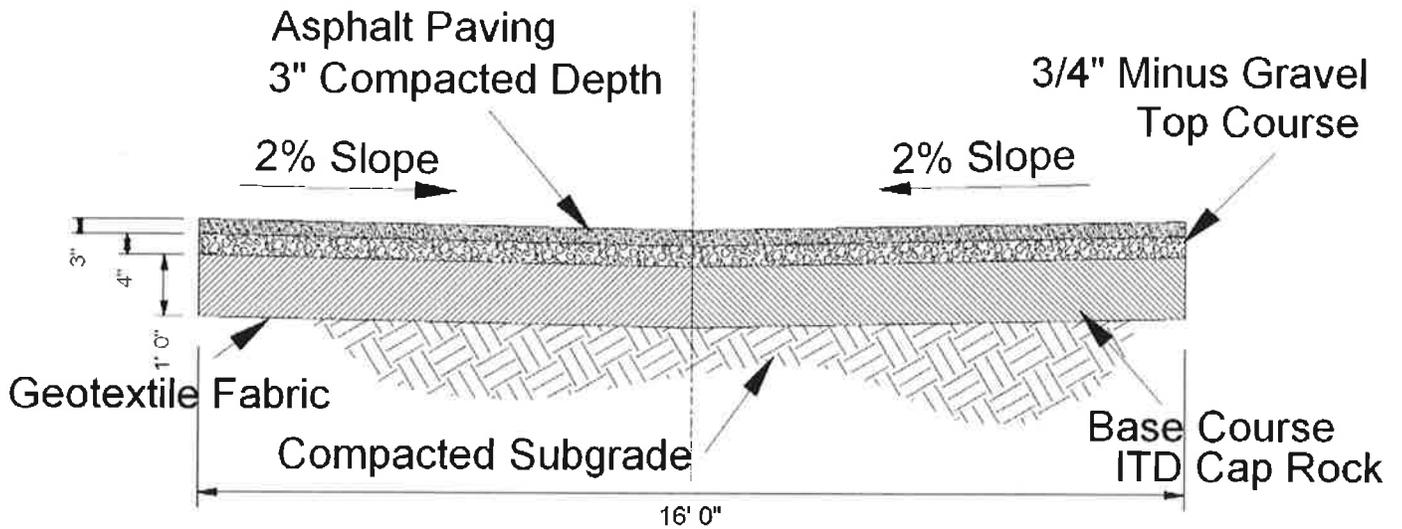
SHEET TITLE:

SIDEWALK CROSS SECTIONS

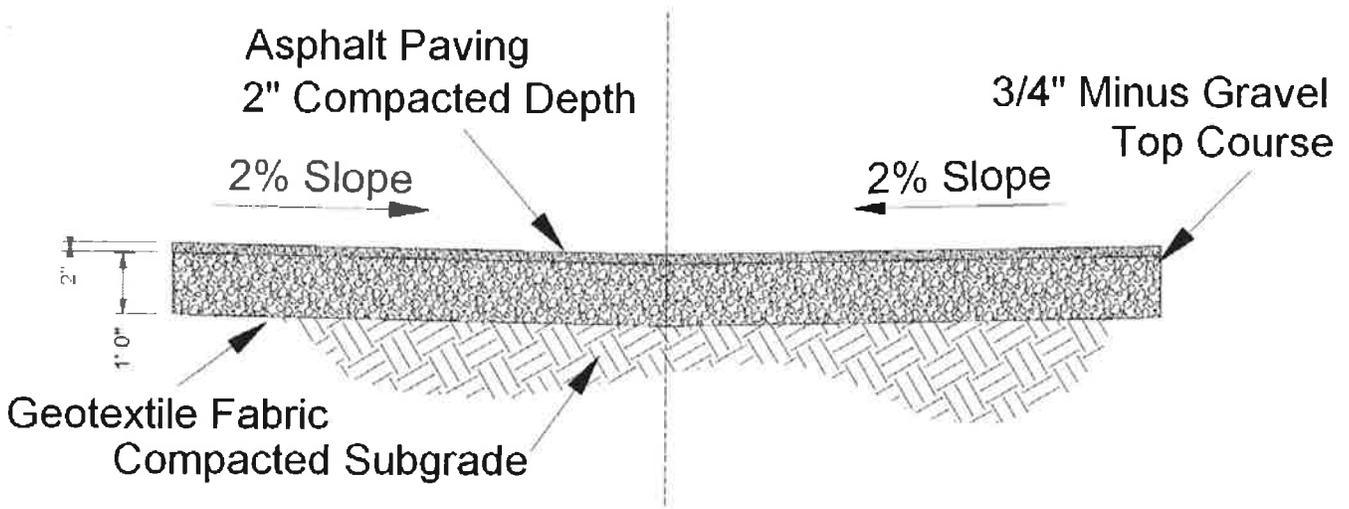
PROJECT: CITY OF SANDPOINT
STANDARRD DRAWINGS

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CITY OF SANDPOINT
PUBLIC WORKS



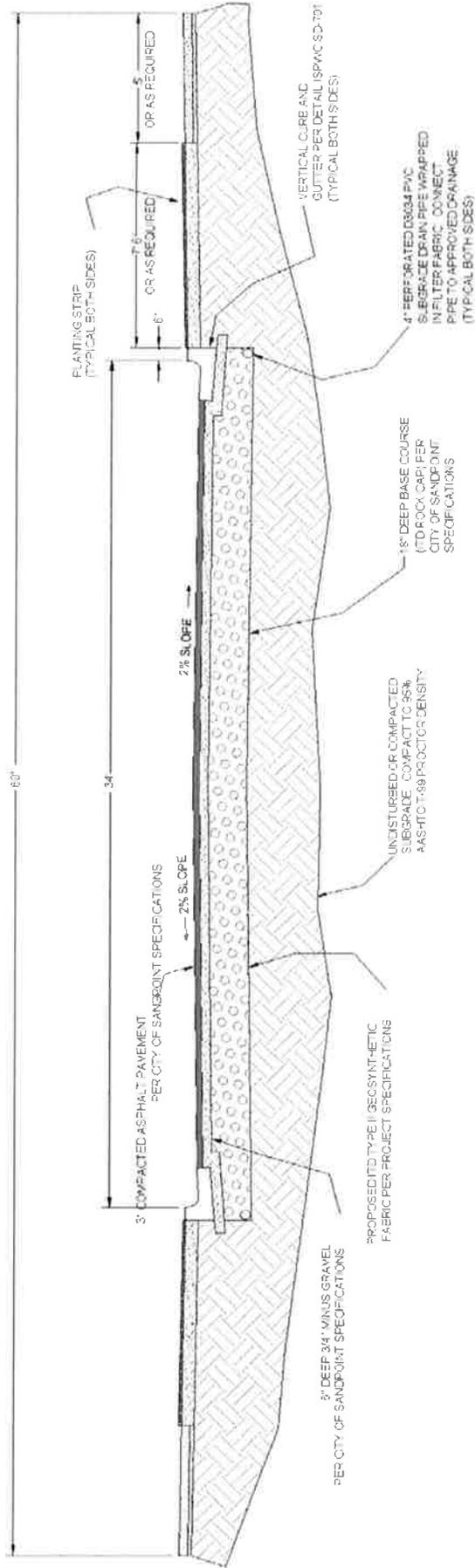
Alley Specification for Commercial Areas



Alley Specification for Residential Areas

SHEET TITLE:			
16' Alley Cross Section			
PROJECT:			
Standard Drawing			
DRAWN:	CHECKED :	DATE :	SHT. 1 OF 1
BWR		4/5/06	
CITY OF SANDPOINT			
PUBLIC WORKS			

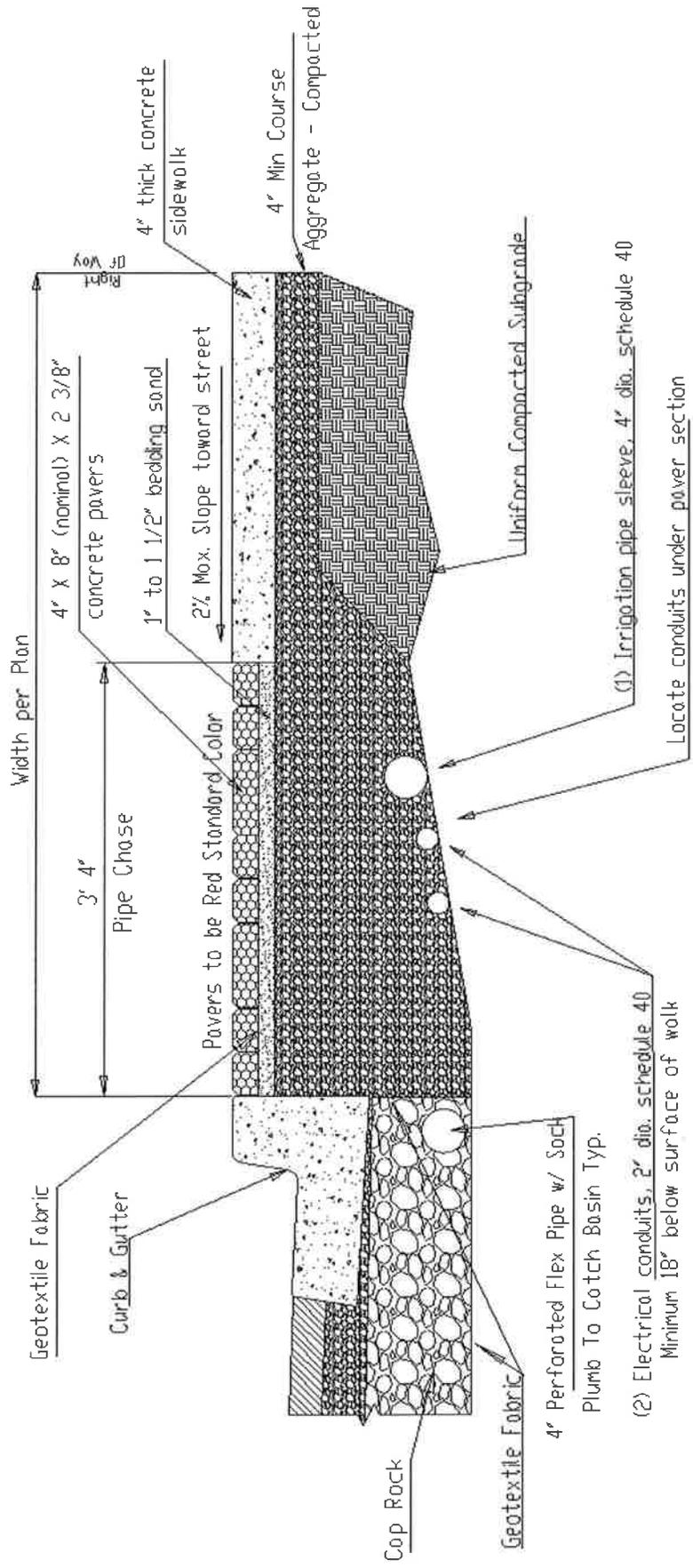
Sandpoint Standard 60' ROW Collector & Local Street Cross Section



SHEET TITLE:	
60' ROW - 34' STREET WIDTH	
PROJECT:	
Collector & Local Street Cross Section	
DRAWN:	CHECKED:
BWR	6/29/06
DATE:	SHT. 1 OF 1
CITY OF SANDPOINT PUBLIC WORKS	

Business Improvement District Standard Sidewalk With Pavers Detail

No Scale

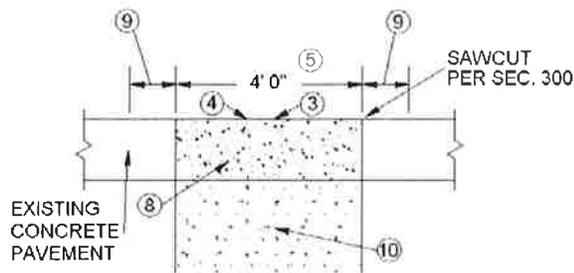


SHEET TITLE: Sidewalk Cross Section	
PROJECT: Business Improvement Dist.	
DRAWN: BWR	CHECKED: DATE: 3/5/14 SHT. _ OF _
CITY OF SANDPOINT PUBLIC WORKS	

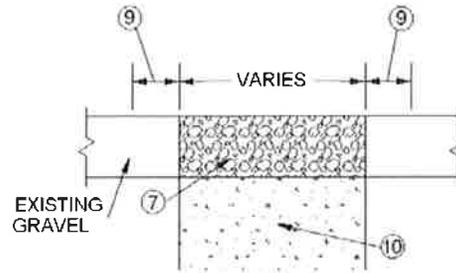


City of Sandpoint
Business Improvement District

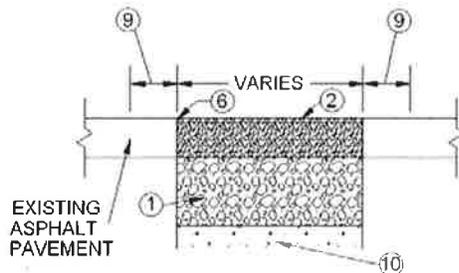
TITLE	CITY OF SANDPOINT	DATE	12-12-2000
	Business Improvement District	REVISE SUBDIVISIONS	
NAME	NA		
DATE	5-12-2006		



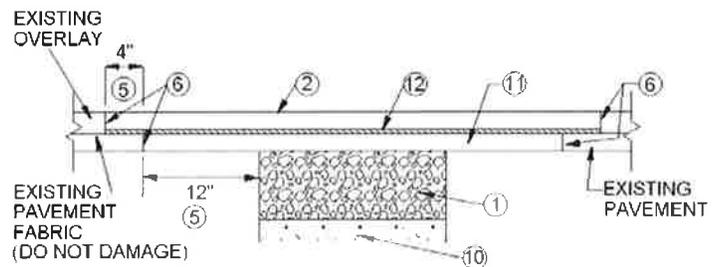
TYPE 'B'
CONCRETE



TYPE 'C'
GRAVEL



TYPE 'P'
ASPHALT



TYPE 'P'
ASPHALT W/ PAVEMENT FABRIC

LEDGEND

- ① 8" OF 3/4" MINUS CRUSHED AGGREGATE BASE (MIN.) OR MATCH EXISTING BASE WHICHEVER IS GREATER
- ② UNLESS OTHERWISE SPECIFIED OR SHOWN ON THE PROJECT DOCUMENTS, MATCH EXISTING ASPHALT MAT THICKNESS (MAX. 6") OR 3" ON RESIDENTIAL STREETS, 4" ON COLLECTORS AND ARTERIALS, WHICHEVER IS GREATER
- ③ PORTLAND CEMENT CONCRETE SHALL BE CLASS 3000 PSI EARLY STRENGTH, AND COMPLY WITH SECTION-706
- ④ KEEP TRAFFIC OFF 72 HOURS, UNLESS OTHERWISE APPROVED BY ENGINEER.
- ⑤ MINIMUM DISTANCES.
- ⑥ CUT ASPHALT IN NEAT STRAIGHT LINE
- ⑦ 3/4" MINUS AGGREGATE SURFACE COURSE (8") OR THICKNESS OF EXISTING GRAVEL, WHICHEVER IS GREATER
- ⑧ THICKNESS EQUALS EXISTING PAVEMENT DEPTH PLUS 2" OF CONCRETE OR PAVEMENT
- ⑨ 12" MIN. OR LOCAL CUTBACK REQUIREMENT WHICHEVER IS GREATER.
- ⑩ COMPACTED TRENCH BACKFILL AS PER S0-301 AND SECTION 306 OF IDAHO STANDARD FOR PUBLIC WORKS CONSTRUCTION
- ⑪ ASPHALT TO EXISTING SHELF (MIN. 2" THICK)
- ⑫ PLACE NEW PAVEMENT FABRIC FULL WIDTH OF ASPHALT PATCH

NOTES:

- (A) REFER TO SECTION-307 IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION FOR MATERIALS AND WORKMANSHIP REQUIREMENTS
- (B) ALL STREET CUTS WILL REQUIRE RESURFACING BY A PAVING MACHINE OR SPREADER BOX. PATCH WIDTHS ARE NEVER TO BE LESS THAN 4" IN WIDTH. LOCATE THE MATCH OF THE NEW TO EXISTING PAVEMENT OUT OF VEHICLE WHEEL PATH OF THE STREET
- (C) WHERE THE STREET SURFACE INCLUDES AND OVERLAY WITH FABRIC, TAKE FOLLOWING ADDITIONAL STEPS:
 - A. OVERLAY ABOVE FABRIC AN ADDITIONAL 4" ON EACH SIDE TO EXPOSE EXISTING FABRIC
 - B. INSTALL NEW FABRIC TO GRADE OF FABRIC
 - C. INSTALL NEW FABRIC FULL WIDTH OF CUT IN ACCORDANCE WITH MANUFACTURERS INSTRUCTIONS
 - D. OVERLAY FABRIC WITH ASPHALT TO FINISH GRADE OF STREET
- (D) TACK ALL COLD JOINT SURFACES WITH EMULSION WHICH HAS BEEN "BROKEN" PRIOR TO PATCHING

SHEET TITLE:

STREET CUTS AND
SURFACE REPAIR DETAILS

PROJECT:

CITY OF SANDPOINT
STANDARD DRAWINGS

DRAWN:

BWR

CHECKED :

DATE :

12/22/06

SHT. 1 OF 1

CITY OF SANDPOINT
PUBLIC WORKS

Table 1
BASIC MIX DESIGN PARAMETERS

Concrete ^(a) Class In Mpa (psi) (28 days)	Minimum Cement Content		Max. Water Cement Ratio	Max. Slump Mm (inch)		Air Content Percent
	Kg/m ³	Lb/CY ^b		Mm	Inch	
30a (4000a)	330	(560)	.44	40 ± 25	(2.5 ± 1.0)	6.5 ± 1.5
30b (4000a)	330	(560)	.44	125	(5)	6.5 ± 1.5
20 (3000)	230	(560)	.49	125	(5)	6.5 ± 1.5
10 (1500)	230	(380)	.60	200	(8)	0-6
Seal Concrete	390	(660)	.60	200	(8)	0-6

Table 1 Footnotes:

- (a) Classes of concrete are the minimum compressive strengths when tested in accordance with applicable tests
- (b) It may not always be possible to produce concrete of the required strength using the minimum cement content. No separate payment will be made for additional cement required to meet minimum strength.
- (c) Sidewalk concrete shall include natural or synthetic fiber mesh, added at the batch plant per the manufacturer's recommendations. (per City Council Resolution 14-04)

Table 2
BASIC MIX DESIGN PARAMETERS
WHEN FLY ASH CONCRETE IS REQUIRED

Concrete ^(a) Class in Mpa (psi) 28 Day	Minimum ^(b) Cement Content		Minimum ^(b) Fly Ash Content		Max. Water to Cement (Plus Fly Ash) Ratio	Slump Mm (inch)	Air Content Percent
	Kg /m ³	Lb./ CY	Kg /m ³	Lb./ CY			
30AF (4000AF)	277	(467)	69	(116)	.42	40 ± 25 (2.5 ± 1.0)	6.5 ± 1.5
30BF (4000BF)	277	(467)	69	(116)	.42	125 (5)	6.5 ± 1.5
20F (3000F)	277	(467)	69	(116)	.47	125 (5) Max.	6.5 ± 1.5
10F (1500F)	188	(317)	47	(79)	.58	200 (8) Max.	0-6
Seal Concrete w/ Fly Ash	326	(550)	82	(138)	.58	200 (8) Max.	0-6

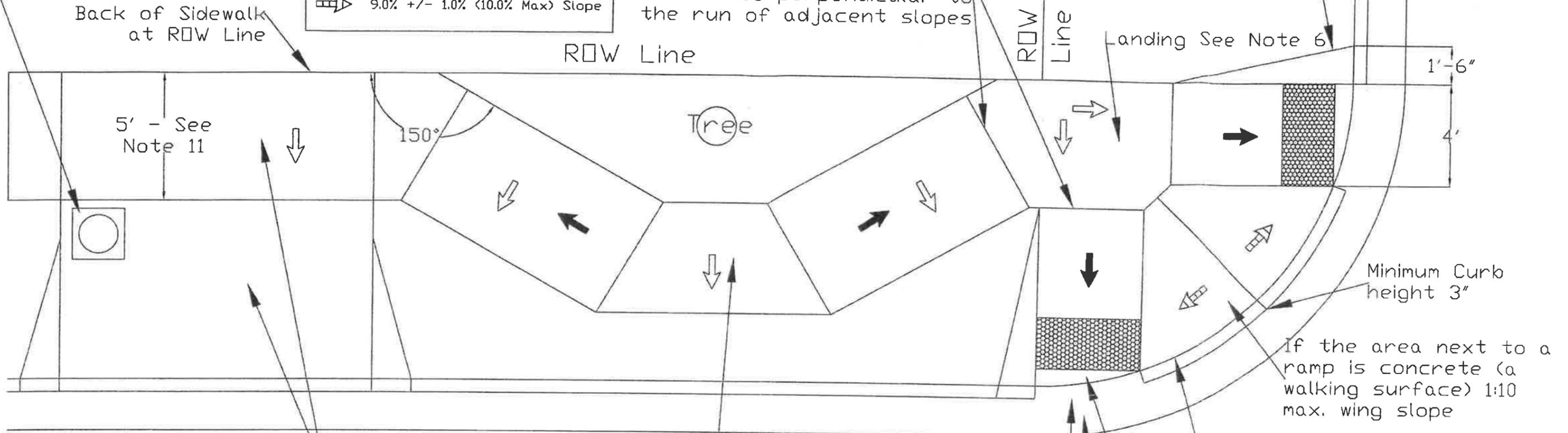
Data from Idaho Standards for Public Works Construction, Division 700-Concrete, Section 703 – Cast-In-Place Concrete

Block out around Utility Poles 3" Min. concrete with expansion joints

Legend	
	1.5% +/- 0.5% (2% Max) Slope
	7.3% +/- 1.0% (8.3% Max) Slope
	9.0% +/- 1.0% (10.0% Max) Slope

Grade breaks at landings shall be perpendicular to the run of adjacent slopes

When the area around ramps is grass (not a walking surface), ramp wing slope may exceed 10%



6" thick concrete w/ #4 rebar 18" OC both directions at drive aprons and adjacent sidewalk

Raise & Meander sidewalk to avoid damage to existing tree roots and future sidewalk damage from tree growth

In ramp throat, transition from curb top slope to counter slope shall not have any vertical lip but shall maintain gutter flow line at not more than 2%.

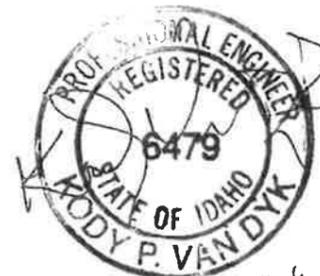
No expansion joint material at ramps, wings or landings

Area between dome panels and curb 2% slope max. in any direction

Gutter counter slope 5% max. at ramp

Notes:

1. A curb height of 6" is assumed. Adjustments may be made for conditions in the field.
2. The detectable warning surface shall be cast iron & installed 24" deep along the full width of the pedestrian ramp. At least one corner of the leading edge of the panel shall be less than 8" from the face of the curb.
3. Ramps shall not have any utilities or structures within the flare, ramp, or landing.
4. Finish concrete with a coarse broom surface perpendicular to travel or slope.
5. Place preformed expansion joint in sidewalk only. Joint filler shall not be placed within 2' of the ramp, flare, or landing.
6. Landings shall have absolute minimum dimensions of 4' X 4' and absolute maximum slopes of 2% in all directions
7. Grade breaks shall be angular and distinct
8. Flares are generally preferred over return curbs to provide gradual curb transitions. Return curbs shall only be installed in locations that are not part of the pedestrian circulation path. Flares that are not part of the pedestrian circulation path may be at any slope (33% preferred max.)
9. Use expansion joints every 20' in sidewalk and at changes in concrete thickness. No expansion joint shall be used within the area of a curb ramp, wings or landings.
10. Block out around utility poles and use expansion joints to avoid cracks and allow pole replacement.
11. Minimum Sidewalk Width shall be per City Resolution 06-53, typically 5', but 6' or wider on Arterials and Collector Streets or with limited planting strips.
12. Concrete mix design shall include natural or synthetic fibermesh per manufacturer's recommendations.



Sheet Title: Std. Sidewalk Details			
Project:			
Drawn: B. W. R.	Checked: K. P. V.	Date: 3/5/14	Sht. 1 of 1
City of Sandpoint, Public Works Approved by Sandpoint City Council 3/5/2014			